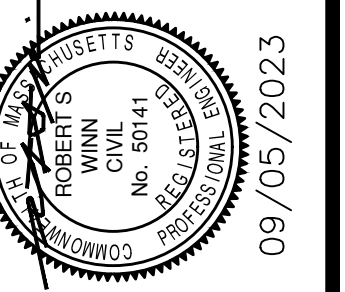
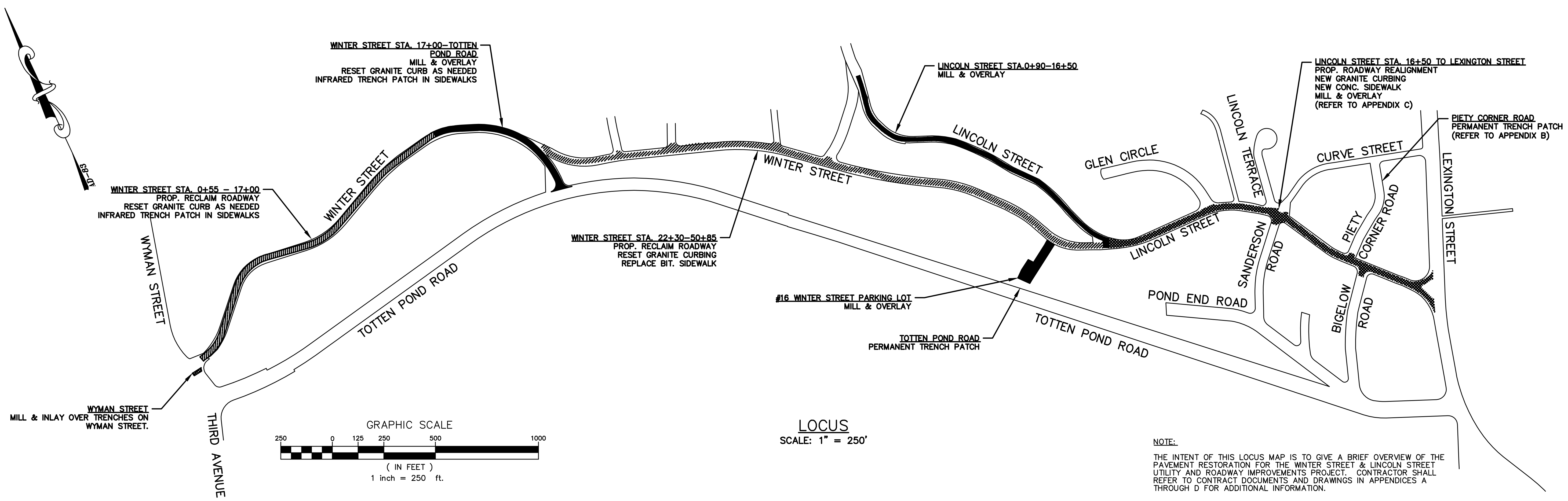


CITY OF WALTHAM ENGINEERING DEPARTMENT  
 WINTER ST & LINCOLN ST IMPROVEMENTS  
 APPENDIX D - ROADWAY PAVING



09/05/2023

DATE: 8/30/2023	REVISION DATE:
SCALE: 1" = 250'	DRAWN BY: JPM
FILE: winterstpaving.dwg	DESIGNED BY: JPM
SHEET 2 OF 15	REVIEWED BY: IAM
	APPROVED BY: RSW



GENERAL NOTES:

- PLAN INFORMATION IS BASED UPON INFORMATION OBTAINED FROM THE CITY OF WALTHAM'S GIS DATA AND IS APPROXIMATE ONLY.
- BUILDING LOCATIONS, AS SHOWN, ARE APPROXIMATE AND FOR REFERENCE PURPOSES ONLY.
- DISTANCES SHALL NOT BE SCALED OFF OF THESE PLANS.
- PRIOR TO THE PROPOSED CONSTRUCTION, THE CONTRACTOR SHALL PROVIDE THE ENGINEER AND CITY WITH A CONSTRUCTION SCHEDULE DELINEATING THE SEQUENCE OF WORK AND ESTIMATED TIME OF COMPLETION FOR EACH SEGMENT OF WORK.
- THE CONTRACTOR SHALL SUBMIT A TRAFFIC MANAGEMENT PLAN TO THE CITY FOR REVIEW AND APPROVAL. THE PLAN SHALL BE IN COMPLIANCE WITH MASSACHUSETTS DEPARTMENT OF TRANSPORTATION (MASSDOT) AND THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).
- THE CONTRACTOR SHALL MAINTAIN CONTINUOUS TRAFFIC FLOW DURING CONSTRUCTION SATISFACTORILY TO THE THE ENGINEER AND THE CITY OF WALTHAM.
- ALL CONSTRUCTION SIGNING SHALL CONFORM TO THE REQUIREMENTS OF MASSDOT AND MUTCD.
- THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR CONSTRUCTION MEANS, METHODS, TECHNIQUES, PROCEDURES, AND FOR SAFETY PRECAUTIONS AND PROGRAMS IN CONNECTION WITH ALL WORK INCLUDED UNDER THIS CONTRACT. THE DRAWINGS DO NOT INCLUDE NECESSARY COMPONENTS FOR CONSTRUCTION SAFETY. THE CONTRACTOR SHALL BE SOLELY RESPONSIBLE FOR PROVIDING AND MAINTAINING ALL SAFETY BARRIERS, WARNING FLASHERS, ETC., AS REQUIRED BY THE CONDUCT OF THE WORK FOR THE PROTECTION OF WORKERS AND NON-WORKERS ALIKE. THE CONTRACTORS ATTENTION IS DIRECTED TO OSHA REQUIREMENTS.
- ALL CONSTRUCTION MATERIAL, DEBRIS, ASPHALT, SOIL, ETC. REMOVED FROM THE SITE SHALL BECOME PROPERTY OF THE CONTRACTOR. THE CONTRACTOR WILL BE RESPONSIBLE FOR TRANSPORTING AND DISPOSING ALL EXCESS MATERIALS IN ACCORDANCE WITH LOCAL, STATE, AND FEDERAL REGULATIONS.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE SITE RESTORATION AND CLEAN UP UPON COMPLETION OF THE PROJECT. DURING THE COURSE OF CONSTRUCTION, ANY DAMAGE BY THE CONTRACTOR TO FENCES, GUARDRAILS, PATHS, STAIRS, PAVEMENT, LANDSCAPING OR VEGETATION SHALL BE REPAIRED OR REPLACED AND RESTORED TO ORIGINAL CONDITION AT NO ADDITIONAL COST TO THE CONTRACT. ANY REPLACEMENT FENCE AND/OR HANDRAILS MUST MATCH EXISTING.
- TRENCH DEWATERING COSTS THROUGHOUT THE DURATION OF THE PROPOSED PROJECT SHALL BE INCLUDED IN THE APPLICABLE BID. A TRENCH DEWATERING PLAN SHALL BE SUBMITTED TO THE CITY ENGINEER FOR APPROVAL.
- ALL CASTINGS, GATE BOXES, HYDRANTS, LIGHT POLES, ETC. DAMAGED DURING CONSTRUCTION SHALL BE SUPPLIED AND REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR RESETTling ALL EXISTING PROPERTY MONUMENTATION THAT IS DISTURBED BY CONSTRUCTION OPERATIONS AT NO EXPENSE TO THE OWNER. THIS WORK IS TO BE DONE BY A PROFESSIONAL LAND SURVEYOR LICENSED IN THE COMMONWEALTH OF MASSACHUSETTS. PROPERTY BOUNDS FOUND ARE SHOWN ON LAYOUT PLANS, THIS MAY NOT BE INCLUSIVE OF ALL BOUNDS THAT EXIST IN THE PROJECT AREA. IF ANY ADDITIONAL BOUNDS ARE FOUND, THE CONTRACTOR SHALL DOCUMENT THE LOCATION AND CONTACT THE ENGINEER.
- THE CONTRACTOR SHALL TAKE SPECIAL CARE NOT TO DAMAGE TREES WITHIN THE CONSTRUCTION AREA UNLESS THEY ARE NOTED TO BE REMOVED.
- THE CONTRACTOR IS RESPONSIBLE TO NOTIFY AND COORDINATE SUPPORT WITH OWNERS OF UTILITY POLES WITHIN 10 FEET OF THE PROPOSED UTILITY PIPE CENTERLINE OR AS DIRECTED BY THE ENGINEER.
- POLICE DETAILS SHALL BE COORDINATED BY THE CONTRACTOR.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREVENTING ANY DEBRIS, SEDIMENT, OR SILTY WATER FROM ENTERING ANY DRAINAGE SYSTEM DURING ALL PHASES OF CONSTRUCTION. THE CONTRACTOR SHALL INSTALL TEMPORARY CATCH BASIN SILT SACKS AS REQUIRED BY THE ENGINEER.
- LEXINGTON ST, TOTTEN POND RD, & WINTER ST (WYMAN ST TO TOTTEN POND RD) ARE RESTRICTED STREETS PER CITY ORDINANCE. NO WORK SHALL OCCUR ON THESE ROADWAYS BETWEEN 7AM-9AM AND 4PM-6PM. LINCOLN ST SHALL NOT BE CLOSED BETWEEN THE HOURS OF 7AM-9AM.
- CONSTRUCTION HOURS SHALL OCCUR BETWEEN THE HOURS OF 7:00 AM AND 5:00 PM, MONDAY THROUGH FRIDAY. WEEKEND & HOLIDAY WORK MUST BE APPROVED AT LEAST 48 HOURS IN ADVANCE BY THE CITY.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL PERMITS REQUIRED FOR THE COMPLETION OF THE WORK.

ROADWAY RECLAMATION NOTES

- THE CONTRACTOR SHALL SAW CUT ALL JOINTS IN THE EXISTING PAVEMENT AREAS WHERE THE PROPOSED PAVEMENT WILL MEET EXISTING PAVEMENT TO REMAIN. ALL JOINTS SHALL PROVIDE A SMOOTH TRANSITION BETWEEN NEW AND OLD PAVEMENTS. IMMEDIATELY AFTER PAVING, ALL NEW JOINTS SHALL BE SANDED AND SEALED. THE COST FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE CONTRACT PRICE.
- ALL TRENCHES WILL BE COMPACTED TO ALLOW PROPER SETTLEMENT. ALL TRENCHES WILL BE COMPACTED TO 95% COMPACTION. INCLUDE PAYMENT UNDER ASSOCIATED ITEM.
- AFTER THE COMPACTION PROCESS IS COMPLETED, TRENCHES FOR TOTTEN POND ROAD, WYMAN STREET, LINCOLN STREET, AND WINTER STREET FROM WYMAN STREET TO TOTTEN POND ROAD SHALL BE TRENCH PAVED AT THE END OF EACH DAY.
- AFTER THE COMPACTION PROCESS IS COMPLETED, THE CONTRACTOR SHALL MAINTAIN TRENCH GRAVEL FLUSH TO EXISTING GRADE AND TRENCH PAVING SHALL BE INSTALLED AT THE END OF EACH WEEK FOR ALL OTHER STREETS.
- THE CONTRACTOR WILL ALLOW THE TRENCHES TO SETTLE THE REQUIRED PERIOD (30 DAY MIN) AS STATED IN THE SPECIFICATIONS PRIOR TO RECLAIMING THE ENTIRE WIDTH OF ROADWAY.
- PRIOR TO RECLAIMING, THE CONTRACTOR SHALL COMPLETE ALL EXCAVATING AND PREPARING SUBGRADE REQUIRED TO PULVERIZE THE PAVEMENT AND SHALL LOWER ALL CASTINGS AS SPECIFIED IN THE CONTRACT SPECIFICATIONS.
- THE ENTIRE ROADWAY SHALL BE RECLAIMED TO A MINIMUM DEPTH OF 16" BELOW THE PROPOSED FINISH GRADE WITHIN THE PROJECT LIMITS. THE EXISTING PAVEMENT SHALL BE PULVERIZED TO THE POINT WHERE NO MATERIAL IS GREATER THAN 3".
- THE CONTRACTOR SHALL RECLAIM THE ENTIRE WIDTH OF EXISTING MATERIAL. THE LIMITS (EDGE OF PAVEMENT) OF THE EXISTING PAVED SURFACE ARE SHOWN IN THE PLAN VIEW OF THESE CONSTRUCTION DRAWINGS.
- AFTER PULVERIZING THE ROADWAY ASPHALT PAVEMENT AND UNDERLYING MATERIALS. THE CONTRACTOR SHALL REMOVE AND STOCKPILE (WINROW) THE RECLAIMED MATERIAL. THE CONTRACTOR SHALL THEN EXCAVATE AND REMOVE THE NECESSARY SUBGRADE MATERIAL IN ORDER TO MEET THE FINAL GRADES OF THE ROADWAY. THE CONTRACTOR SHALL THEN PLACE, GRADE AND COMPACT THE EXISTING RECLAIMED BASE COURSE TO A 12" DEPTH AS SHOWN ON THE TYPICAL ROADWAY CROSS SECTION. THE SUBBASE SHALL THEN BE FINE GRADED AND COMPACTED TO ALLOW FOR THE PLACEMENT OF: 2-1/2" BASE COURSE INTERMEDIATE COURSE PAVEMENT (SIC-19.0-TABLE 460.10-1) AND 1-1/2" SURFACE COURSE (SSC-9.5-TABLE 460.10-1) AND ACCORDING TO MASSDOT SECTION 460-"HOT MIX ASPHALT PAVEMENT FOR LOCAL STREETS"
- AFTER PULVERIZING THE EXISTING IN PLACE ASPHALT AND UNDERLYING MATERIAL (TOTAL OF 16" DEPTH), THE CONTRACTOR SHALL PLACE, GRADE AND COMPACT THE EXISTING RECLAIMED BASE COURSE TO A 12" DEPTH AS SHOWN ON THE TYPICAL ROADWAY CROSS SECTION.
- THE CONTRACTOR SHALL GRADE THE EXISTING RECLAIMED SUBBASE MATERIAL OR GRAVEL BORROW MATERIAL TO ALLOW THE FINAL PAVEMENT SURFACE TO MATCH THE EXISTING EDGE OF PAVEMENT GRADES UNLESS OTHERWISE NOTED. THE RECONSTRUCTION OF THE ROADWAY SHALL ALSO BE IN ACCORDANCE WITH THE TYPICAL CROSS SECTION DETAIL. ANY GRADING MODIFICATIONS SHALL DIRECT DRAINAGE TOWARDS THE APPROPRIATE AREAS.
- ALL PROPOSED CUTS AND FILLS REQUIRED TO GRADE THE RECLAIMED MATERIAL TO A 12" DEPTH SHALL BE INCLUDED FOR PAYMENT UNDER THE APPROPRIATE ITEM.
- ALL DRAINAGE AND UTILITY CASTINGS SHALL BE LOWERED OR REMOVED AND PLATED PRIOR TO RECLAIMING THE ROADWAY. ALL STRUCTURES MUST BE LOWERED TO A DEPTH OF 6 INCHES BELOW THE BOTTOM OF THE PROPOSED RECLAIMED BASE COURSE.
- THE CONTRACTOR SHALL FINE GRADE THE EXISTING RECLAIMED BASE COURSE MATERIAL NO MORE THAN 24 HOURS PRIOR TO THE PLACEMENT OF THE 2 1/2" BASE COURSE PAVEMENT. ALL GRADING, COMPACTION AND DUST CONTROL ASSOCIATED WITH FINE GRADING TO BE INCLUDED IN THE APPROPRIATE RECLAIM ITEM.
- THE CONTRACTOR SHALL STOCKPILE AND RETAIN SUFFICIENT SURPLUS SUBBASE AND RECLAIMED

ROADWAY RECLAMATION NOTES CONT.

- PAVEMENT SUBBASE MATERIALS TO USE AS NEEDED IN THE ENTIRE PROJECT AREA. THE COSTS ASSOCIATE WITH THE EXCAVATION, PLACEMENT AND DISPOSAL OF SURPLUS SUBBASE MATERIAL SHALL BE INCLUDED IN THE APPROPRIATE ITEM. NO ADDITIONAL PAYMENT FOR PLACEMENT SHALL BE MADE. SURPLUS SUBBASE AND RECLAIMED PAVEMENT SUBBASE MATERIAL SHALL BE USED ONSITE PRIOR TO GRAVEL BORROW MATERIAL. ANY EXCESS RECLAIMED MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND SHALL BE REMOVED AND DISPOSED OF IN ACCORDANCE WITH LOCAL, STATE AND FEDERAL REGULATIONS. NO ADDITIONAL PAYMENT FOR DISPOSAL SHALL BE MADE.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR COMPACTION TESTING. TESTING SHALL BE PERFORMED AT INTERVALS OF 100 FEET ALONG THE ROADWAY. SEE SPECIFICATION SECTION 02250 FOR COMPACTION CONTROL AND TESTING.
- PRIOR TO COMPLETING FINAL GRADING OF THE RECLAIMED BASE COURSE, THE ENGINEER SHALL REVIEW GRADES TO DETERMINE THAT SUFFICIENT CROSS SLOPES AND POSITIVE DRAINAGE FLOWS HAVE BEEN MAINTAINED. IF GRADES NEED TO BE ADJUSTED, THE CONTRACTOR SHALL REGRADE AS DIRECTED.
- CROSS SLOPES AT CATCH BASINS SHALL BE ADJUSTED AS NECESSARY TO ASSURE PROPER DRAINAGE.
- CONTRACTOR SHALL CONTROL DUST DURING CONSTRUCTION USING CALCIUM CHLORIDE AS NECESSARY.
- DRAINAGE STRUCTURES SHALL BE ADJUSTED OR REMODELED AS REQUIRED TO MEET GRADE.
- SHOULDERS OF DRIVEWAY AND PARKING AREAS SHALL BE GRADED FOR A SMOOTH TRANSITION FROM THE PROPOSED EDGE OF PAVEMENT/CURB TO THE EXISTING GRADE.
- ALL STRUCTURES SHALL BE LOWERED PRIOR TO RECLAIMING AND THEN RAISED TO FINISHED GRADE ONCE BINDER IS PLACED.

GENERAL PAVING NOTES:

- THE CONTRACTOR SHALL SAW CUT ALL JOINTS IN THE EXISTING PAVEMENT AREAS WHERE THE PROPOSED PAVEMENT WILL MEET EXISTING PAVEMENT TO REMAIN. ALL JOINTS SHALL PROVIDE A SMOOTH TRANSITION BETWEEN NEW AND OLD PAVEMENTS. IMMEDIATELY AFTER PAVING, ALL NEW JOINTS SHALL BE SANDED AND SEALED. THE COST FOR THIS WORK SHALL BE CONSIDERED INCLUDED IN THE CONTRACT PRICE.
- THE CONTRACTOR SHALL RESET ALL WATER, SEWER, DRAIN, GAS, ELECTRIC, AND TELEPHONE FRAMES AND COVERS/GRATES AND ANY OTHER STRUCTURES, SIGNS, ETC. NECESSARY TO INSTALL THE PROPOSED PAVEMENT TO THE PROPOSED FINISH GRADE ELEVATION. ADJUSTMENT OF STRUCTURES WITHIN THE ROADWAY SHALL BE PAID FOR UNDER THE APPROPRIATE BID ITEM WITHIN THE CONTRACT. ALL OTHER ADJUSTMENTS SHALL BE CONSIDERED INCIDENTAL AND INCLUDED WITHIN THE CONTRACT PRICE.
- FINAL ROADWAY MILLING & PAVING SHALL BE CONDUCTED AFTER A MINIMUM 90 DAY SETTLEMENT PERIOD. THE CONTRACTOR SHALL BE RESPONSIBLE TO MAINTAIN ALL TRENCHES FOR THE DURATION OF THE SETTLEMENT PERIOD AT NO ADDITIONAL COST TO THE CONTRACT.
- THE CONTRACTOR SHALL MILL THE EXISTING ROADWAY TO A DEPTH OF 1.5 INCHES (OR AS NOTED). CONTRACTOR SHALL PLACE TOP COURSE PAVEMENT TO A DEPTH OF 1.5 INCHES (OR AS NOTED) COMPACTED TO RESTORE THE ROADWAY TO ITS ORIGINAL GRADE.
- ALL CASTINGS, GATE BOXES, ETC. DAMAGED DURING CONSTRUCTION SHALL BE SUPPLIED AND REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE CONTRACT.
- CONTRACTOR WILL BE RESPONSIBLE TO RE-STRIPE THE ROADWAYS UPON COMPLETION OF FINAL ROADWAY RESTORATION WITH THERMOPLASTIC MARKINGS. SEE STRIPING PLANS ATTACHED AS AN APPENDIX TO THESE BID DOCUMENTS.