

THE CITY OF WALTHAM  
MASSACHUSETTS

PURCHASING DEPARTMENT

**Embassy Parking Garage Renovations, 2024**

**ADDENDUM NO. 5**

April 10, 2024

**CHANGES, CORRECTIONS AND CLARIFICATIONS**

The attention of bidders submitting proposals for the above subject project is called to the following addendum to the specifications. The items set forth herein, whether of omission, addition, substitution, or clarification are all to be included in and form a part of the proposal submitted. THE NUMBER OF THIS ADDENDUM (NO. 5) MUST BE ACKNOWLEDGED IN THE REVISED FORM FOR GENERAL BID SECTION 00 60 01.

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**ITEM 1: Answers to Posed Questions**

**Q1.** Can the bid date be extended by one week? Shoring Design engineer has requested as built drawings/existing spandrel wall shop drawings detail to work on shoring design for demolition and rebuild? Thus, can bid date be extended by one week?

**A1.** The City of Waltham has a tight timeline for requesting the funding for this project, General Bids are due at 2:00 pm on April 16<sup>th</sup>, 2024.

**Q2.** Please provide following details for Concrete Spandrel Wall:

- a. As Built/Shop Drawings of existing concrete spandrel wall at ramp.
- b. Existing connection detail of precast ramp tees to Spandrel Wall and spacing of connection.
- c. Existing Connection Detail of concrete spandrel wall at ends on ramps on underside.
- d. As per detail 7/S1-04 – Concrete Ramp Spandrel – Please provide details of tee to wall connection which is missing as per picture below? Please confirm if contractor to provide new connection with assembly or can reuse existing one?

**A2.**

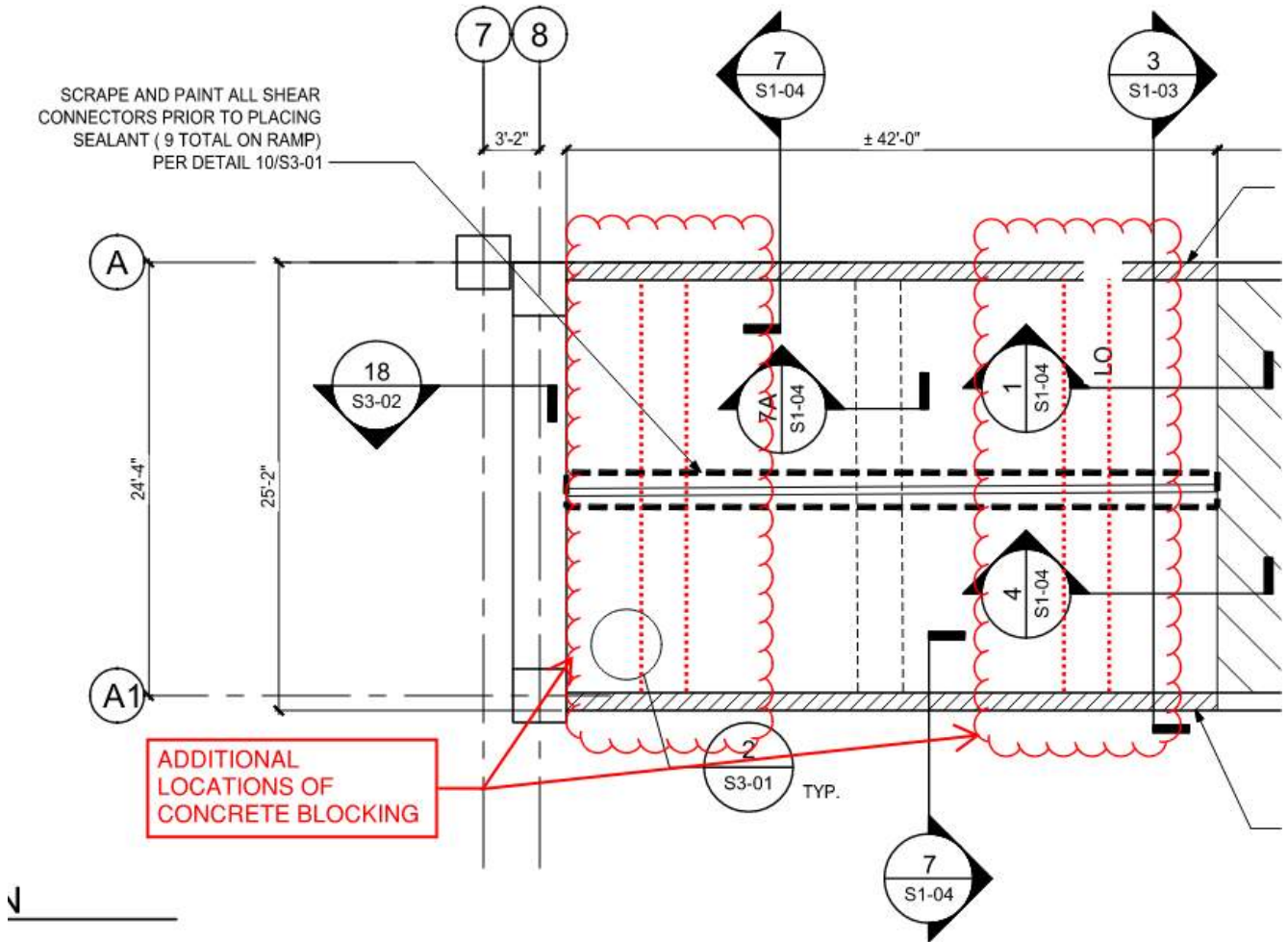
**A2.a** – There are no existing As-Built or shop drawings for the concrete ramp spandrel.

**A2.b** – The connection of the precast ramp tee to the existing concrete spandrel wall is a welded bar between two embedded plates, similar to details 10/S3-01 and 11/S3-01. This

connection occurs at approximately 4'-0" on center. See attached photos for more information.

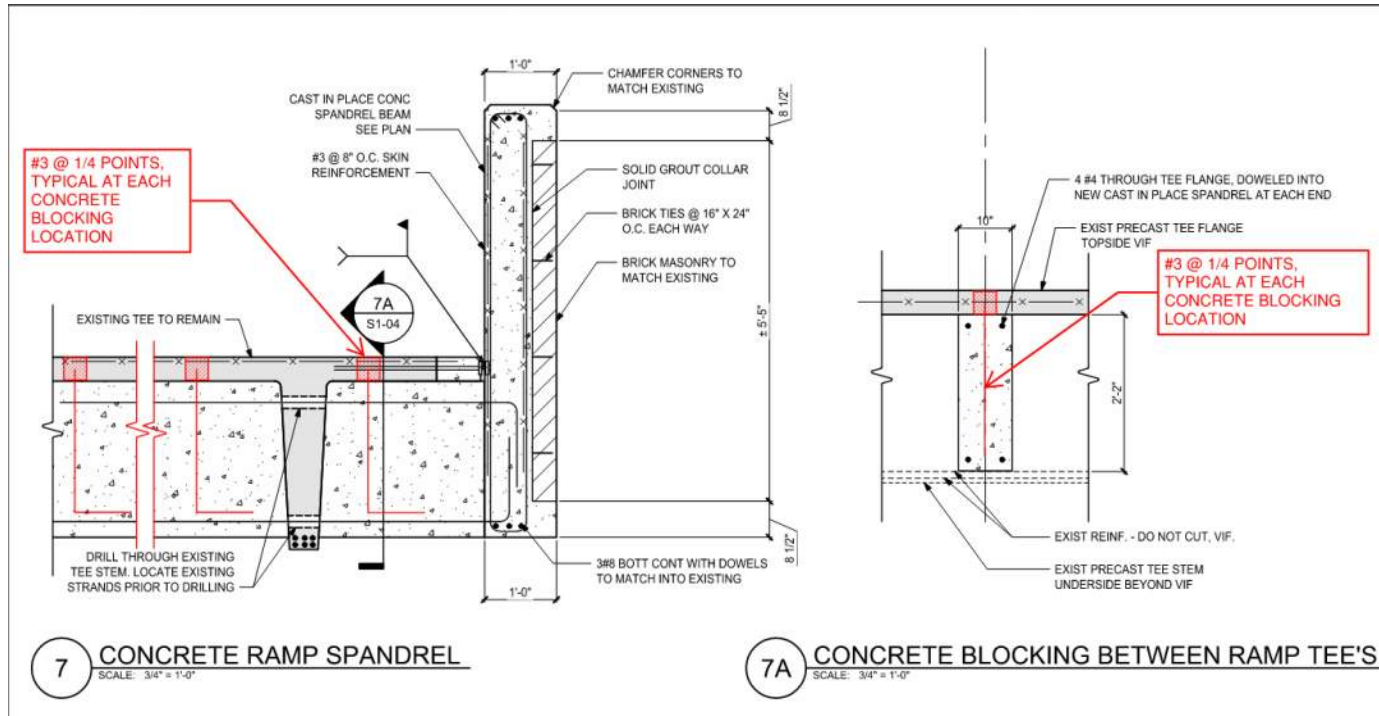
A2.c – One end of the existing ramp spandrel wall is bearing on the vertical concrete ramp wall. The top end of the ramp is bearing on a concrete L-Beam. See 2/S1-03.

A2.d – The connection of the Concrete Spandrel Wall to the tee will be achieved via the reinforcing bars from the Concrete Blocking per detail 7A/S1-04. This will occur at 1/4 points on the ramp (See 1/S1-03 Plan Below).



**Q3.** As per detail 7A/S1-04 – Concrete Blocking Between Ramp Tee’s – please confirm there is no doweling needed at underside of existing precast tee flange while building up the new concrete blocking between ramp tee’s.

**A3.** See revised detail 7/S1-04 & 7A/S1-04 below.



END OF ADDENDUM NO. 5



